

PC RESOLUTION NO. 2025-11

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF TEMECULA RECOMMENDING THAT THE CITY COUNCIL ADOPT AN ORDINANCE ENTITLED “AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF TEMECULA AMENDING TITLE 17 OF THE TEMECULA ZONING CODE TO ADD A NEW ARTICLE XVII (BEDFORD COURT PLANNED DEVELOPMENT OVERLAY DISTRICT 16) TO CHAPTER 17.22 (PLANNED DEVELOPMENT OVERLAY ZONING DISTRICT) TO REVISE THE ZONING DESIGNATION FROM HIGHWAY TOURIST (HT) TO PLANNED DEVELOPMENT OVERLAY 16 (PDO-16) ON A 1.88 ACRE SITE GENERALLY LOCATED ON THE SOUTHWEST CORNER OF BEDFORD COURT APPROXIMATELY 160 FEET SOUTHWEST OF THE TEMECULA PARKWAY AND BEDFORD COURT INTERSECTION (APN 922-210-042) AND AMENDING THE TEMECULA ZONING MAP”

Section 1. Procedural Findings. The Planning Commission of the City of Temecula does hereby find, determine and declare that:

A. On May 4, 2023, the applicant submitted Planning Application PA23-0197, a Development Plan, PA23-0198, a Conditional Use Permit to allow a car wash, PA23-0204, a Tentative Parcel Map to create two parcels from one existing parcel, and PA23-0280, a Zone Change/Planned Development Overlay. On September 17, 2024 the applicant submitted PA24-0348, a Conditional Use Permit to allow for a drive-thru. Taken together, the applications will permit the development and operation of a commercial center consisting of two structures that will house a carwash and coffee shop. These applications (collectively “Project”) were filed in a manner in accord with the City of Temecula General Plan and Development Code.

B. The Project was processed including, but not limited to a public notice, in the time and manner prescribed by State and local law.

C. The Planning Commission, at a regular meeting, considered the Project and environmental review on April 16, 2025, at a duly noticed public hearing as prescribed by law, at which time the City staff and interested persons had an opportunity to and did testify either in support or in opposition to this matter.

D. All legal preconditions to the adoption of this Resolution have occurred.

Section 2. Further Findings. The Planning Commission, in recommending approval the Zone Change/Planned Development Overlay Application No. PA23-0280, hereby finds, determines and declares that: Zone Change/Planned Development Overlay Planning Application No. PA23-0280 is consistent with the General Plan for The City of Temecula and with all applicable requirements of State law and other Ordinances of the City:

A. The proposed Ordinance is in conformance with the General Plan for Temecula and with all applicable requirements of State law and other Ordinances of the City;

The proposed Zone Change/Planned Development Overlay Ordinance conforms to the City of Temecula General Plan Land Use Element. The proposed Zone Change/Planned Development Overlay Ordinance directly responds to Goal 1 Policies LU-1.1, and LU-1.6 of the General Plan Land Use Element.

In addition to employing the City's planned development overlay zoning district tool, the proposed project is also consistent with the above General Plan Land Use Element goals and policies in that it: provides a mix of community-serving commercial uses; has been planned and evaluated for consistency with the General Plan and its implementing programs (e.g., the planned development overlay zoning district); and has been designed to minimize impacts on surrounding land uses and infrastructure through required design guidelines, building orientation, circulation and access improvements, and other features and requirements of proposed Zone Change/Planned Development Overlay 16. Additionally, mitigation measures are identified in the Mitigation Monitoring and Reporting Program to further reduce the potential for impacts to surrounding uses and infrastructure.

B. The proposed Ordinance is consistent with the Municipal Code and Development Code for the City of Temecula;

The proposed Zone Change/Planned Development Overlay Ordinance is internally consistent with the Municipal Code and the Development Code in terms of referencing key components of these documents.

Section 3. Environmental Compliance Findings. The Planning Commission hereby makes the following environmental findings and determinations in connection with the approval of the Zone Change/Planned Development Overlay Application No. PA23-0280:

A. The Project was processed, including but not limited to all public notices, in the time and manner prescribed by State and local law, including the California Environmental Quality Act, Public Resources Code 21000, et seq. and the California Environmental Quality Act Guidelines, 14 Cal. Code Regs 15000 et seq. (collectively referred to as "CEQA").

B. The City contracted with De Novo Planning Group (De Novo) for the independent preparation of an Initial Study to analyze the potential environmental effects of the Project. Based on the information contained in the Initial Study, De Novo and City staff concluded that the Project could have a significant effect on the environment, but that mitigation measures could be implemented to reduce such impacts to a less than significant level. Based upon this determination, De Novo prepared, and City staff concurred in, a Draft Mitigated Negative Declaration ("Draft MND") in accordance with CEQA Section 21080(c) and Section 15070 of the State CEQA Guidelines.

C. On April 16, 2025, the Planning Commission held a duly noticed public hearing on the Project and considered the Mitigated Negative Declaration and Mitigation Monitoring and

Reporting Program, at which time the City staff and interested persons had an opportunity to, and did testify either in support of or opposition to this matter.


D. Following consideration of the entire record before it at the public hearing and due consideration of the Project the Planning Commission adopted Resolution No. 2025- “A RESOLUTION OF THE OF THE PLANNING COMMISSION OF THE CITY OF TEMECULA RECOMMENDING THAT THE CITY COUNCIL ADOPT THE FINAL MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM FOR THE BEDFORD COURT PROJECT CONSISTING OF APPROXIMATELY 1.88 ACRES GENERALLY LOCATED ON THE SOUTHWEST SIDE OF BEDFORD COURT APPROXIMATELY 160 FEET SOUTHWEST OF THE TEMECULA PARKWAY AND BEDFORD COURT INTERSECTION (APN: 922-210-042)”.

Section 4. Recommendation. The Planning Commission of the City of Temecula hereby recommends that the City Council adopt an Ordinance entitled, “AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF TEMECULA AMENDING TITLE 17 OF THE TEMECULA ZONING CODE TO ADD A NEW ARTICLE XVII (BEDFORD COURT PLANNED DEVELOPMENT OVERLAY DISTRICT 16) TO CHAPTER 17.22 (PLANNED DEVELOPMENT OVERLAY ZONING DISTRICT) TO REVISE THE ZONING DESIGNATION FROM HIGHWAY TOURIST (HT) TO PLANNED DEVELOPMENT OVERLAY 16 (PDO-16) ON A 1.88 ACRE SITE GENERALLY LOCATED ON THE SOUTHWEST CORNER OF BEDFORD COURT APPROXIMATELY 160 FEET SOUTHWEST OF THE TEMECULA PARKWAY AND BEDFORD COURT INTERSECTION (APN: 922-210-042) AND AMENDING THE TEMECULA ZONING MAP”. The Planning Commission recommends that the City Council amend the Zoning Code in substantially the same form as attached to this Resolution as Exhibit “A”, and amend the Temecula Zoning Map in substantially the same form as shown in Exhibit “B”.

PASSED, APPROVED AND ADOPTED by the City of Temecula Planning Commission this 16th day of April, 2025.


Lanae Turley-Trejo, Chair

ATTEST:



Matt Peters
Secretary

[SEAL]

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE)ss
CITY OF TEMECULA)

I, Matt Peters, Secretary of the Temecula Planning Commission, do hereby certify that the foregoing PC Resolution No. 2025-11 was duly and regularly adopted by the Planning Commission of the City of Temecula at a regular meeting thereof held on the 16th day of April 2025, by the following vote:

AYES:	4	PLANNING COMMISSIONERS:	Hagel, Turley-Trejo, Watson, Watts
NOES:	0	PLANNING COMMISSIONERS:	None
ABSTAIN:	0	PLANNING COMMISSIONERS:	None
ABSENT:	1	PLANNING COMMISSIONERS:	Solis



Matt Peters
Secretary

ORDINANCE NO. 2025-

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF TEMECULA AMENDING TITLE 17 OF THE TEMECULA ZONING CODE TO ADD A NEW ARTICLE XVII (BEDFORD COURT PLANNED DEVELOPMENT OVERLAY DISTRICT 16) TO CHAPTER 17.22 (PLANNED DEVELOPMENT OVERLAY ZONING DISTRICT) TO REVISE THE ZONING DESIGNATION FROM HIGHWAY TOURIST (HT) TO PLANNED DEVELOPMENT OVERLAY 16 (PDO-16) ON A 1.88 ACRE SITE GENERALLY LOCATED ON THE SOUTHWEST CORNER OF BEDFORD COURT APPROXIMATELY 160 FEET SOUTHWEST OF THE TEMECULA PARKWAY AND BEDFORD COURT INTERSECTION (APN: 922-210-042) AND AMENDING THE TEMECULA ZONING MAP

THE CITY COUNCIL OF THE CITY OF TEMECULA DOES HEREBY ORDAIN AS FOLLOWS:

Section 1. Procedural Findings. The City Council of the City of Temecula does hereby find, determine and declare that:

A. On May 4, 2023, the applicant submitted Planning Application PA23-0197, a Development Plan, PA23-0198, a Conditional Use Permit to allow a car wash, PA23-0204, a Tentative Parcel Map to create two parcels from one existing parcel, and PA23-0280, a Zone Change/Planned Development Overlay. On September 17, 2024 the applicant submitted PA24-0348, a Conditional Use Permit to allow for a drive-thru. Taken together, the applications will permit for the development and operation of a commercial center consisting of two structures that will house a carwash and coffee shop. These applications (collectively “Project”) were filed in a manner in accord with the City of Temecula General Plan and Development Code.

B. The Project was processed including, but not limited to a public notice, in the time and manner prescribed by State and local law, including the California Environmental Quality Act.

C. The Project was processed, including but not limited to all public notices, in the time and manner prescribed by State and local law, including the California Environmental Quality Act, Public Resources Code 21000, et seq. and the California Environmental Quality Act Guidelines, 14 Cal. Code Regs 15000 et seq. (collectively referred to as “CEQA”).

D. The City contracted with De Novo Planning Group (De Novo) for the independent preparation of an Initial Study to analyze the potential environmental effects of the Project. Based on the information contained in the Initial Study, De Novo concluded, and City staff concurred, that the Project could have a significant effect on the environment, but that mitigation measures could be implemented to reduce such impacts to a less than significant level. Based upon this determination, De Novo prepared, and City staff concurred in, a Draft Mitigated Negative Declaration (“Draft MND”) in accordance with CEQA Section 21080(c) and Section 15070 of the

State CEQA Guidelines. The Draft MND was subsequently circulated, comments were received, and responses to comments were prepared resulting in the preparation of the Final Mitigated Negative Declaration (“Final MND”).

E. On April 16, 2025, the Planning Commission held a duly noticed public hearing on the Project and considered the Final MND and Mitigation Monitoring and Reporting Program, at which time the City staff and interested persons had an opportunity to, and did testify either in support of or opposition to this matter.

F. Following consideration of the entire record of information received at the public hearings and due consideration of the proposed Project, the Planning Commission adopted Resolution Nos. 2025-_____, recommending that the City Council approve the Project and the Final MND and Mitigation Monitoring and Reporting Program.

G. On May 13, 2025, the City Council of the City of Temecula considered the Project, the Final MND, and Mitigation Monitoring and Reporting Program at a duly noticed public hearing at which time all interested persons had an opportunity to and did testify either in support or in opposition to this matter. The Council considered all the testimony and any comments received regarding the Project, Final MND, and Mitigation Monitoring and Reporting Program prior to and at the public hearing.

H. Following consideration of the entire record before it at the public hearing and due consideration of the Project the City Council adopted Resolution No. 2025- “A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF TEMECULA ADOPTING THE FINAL MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM FOR THE BEDFORD COURT PROJECT CONSISTING OF APPROXIMATELY 1.88 ACRES GENERALLY LOCATED ON THE SOUTHWEST SIDE OF BEDFORD COURT APPROXIMATELY 160 FEET SOUTHWEST OF THE TEMECULA PARKWAY AND BEDFORD COURT INTERSECTION (APN: 922-210-042)”.

I. All legal preconditions to the adoption of this Resolution have occurred.

Section 2. Legislative Findings. The City Council in approving the hereby makes the following findings regarding the zone change:

A. The proposed Ordinance is in conformance with the General Plan for Temecula and with all applicable requirements of State law and other Ordinances of the City;

The proposed Zone Change/Planned Development Overlay Ordinance conforms to the City of Temecula General Plan Land Use Element. The proposed Zone Change/Planned Development Overlay Ordinance directly responds to Goal 1 Policies LU-1.1, and LU-1.6 of the General Plan Land Use Element.

In addition to employing the City’s planned development overlay zoning district tool, the proposed project is also consistent with the above General Plan Land Use Element goals and policies in that it: provides a mix of community-serving commercial uses; has been planned and evaluated for consistency with the General Plan and its implementing

programs (e.g., the planned development overlay zoning district); and has been designed to minimize impacts on surrounding land uses and infrastructure through required design guidelines, building orientation, circulation and access improvements, and other features and requirements of proposed Planned Development Overlay 16. Additionally, mitigation measures are identified in the Mitigation Monitoring and Reporting Program to further reduce the potential for impacts to surrounding uses and infrastructure.

B. The proposed Ordinance is consistent with the Municipal Code and Development Code for the City of Temecula;

The proposed Zone Change/Planned Development Overlay Ordinance is internally consistent with the Municipal Code and the Development Code in terms of referencing key components of these documents.

Section 3. Zoning Code Amendment. The City Council hereby amends Chapter 17.22 (PLANNED DEVELOPMENT OVERLAY ZONING DISTRICT (PDO-16)) of Title 17 (Zoning) of the Temecula Municipal Code by adding a new Article XVII, entitled “Bedford Court Planned Development Overlay District 16” to read as provided in Exhibit “A”, attached to this Ordinance and incorporated herein as thought set forth in full.

Section 4. Zoning Map Amendment. Based upon the findings contained in Section 2 of this Ordinance, the City Council hereby amends the Official Zoning Map of the City of Temecula to change the zoning classification for the property located on 1.88 acres generally located at the southern end of Bedford Court approximately 160 feet southwest of the Temecula Parkway and Bedford Court intersection (APN: 922-210-042) from Highway Tourist (HT) to Planned Development Overlay (Bedford Court Planned Development Overlay-16). The amended Zoning Map is attached as Exhibit “B” to this Ordinance and is incorporated herein by this reference as though set forth in full.

Section 6. Severability. If any portion, provision, section, paragraph, sentence, or word of this Ordinance is rendered or declared to be invalid by any final court action in a court of competent jurisdiction, or by reason of any preemptive legislation, the remaining portions, provisions, sections, paragraphs, sentences, and words of this Ordinance shall remain in full force and effect and shall be interpreted by the court so as to give effect to such remaining portions of the Ordinance.

Section 7. Effective Date. This Ordinance shall take effect thirty (30) days after its adoption.

Section 8. Notice of Adoption. The City Clerk shall certify to the adoption of this Ordinance and cause it to be published in the manner required by law.

PASSED, APPROVED, AND ADOPTED by the City Council of the City of Temecula
this 13th day of May, 2025.

Brenden Kalfus, Mayor

ATTEST:

Randi Johl, City Clerk

[SEAL]

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss
CITY OF TEMECULA)

I, Randi Johl, City Clerk of the City of Temecula, do hereby certify that the foregoing Ordinance No. 2025- was duly introduced and placed upon its first reading at a meeting of the City Council of the City of Temecula on the 13th day of May, 2025, and that thereafter, said Ordinance was duly adopted by the City Council of the City of Temecula at a meeting thereof held on the 13th day of May, 2025, by the following vote:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSTAIN: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

Randi Johl, City Clerk

Bedford Court PDO-16

Prepared For:

City of Temecula
41000 Main Street
Temecula, California 92590
951.694.6444

Property Owner:

Catalyst Commercial Group
38605 Calistoga Drive, Suite 150
Murrieta, California 92563
951.395.0000
Mark Cooper

Prepared By:

Matthew Fagan Consulting Services, Inc.
42011 Avenida Vista Ladera
Temecula, CA 92591
951.265.5428
Matthew Fagan
Angie Douvres

In conjunction with:

MMA Architecture
Conceptual Design and Planning Company
Kimley-Horn and Associates, Inc.

April 2025

Table of Contents

17.22.002 TITLE	1
17.22.004 PURPOSE	1
17.22.006 RELATIONSHIP WITH THE MUNICIPAL CODE AND CITYWIDE DESIGN STANDARDS	1
17.22.008 USE REGULATIONS	1
17.22.010 PDO-16 DESIGN STANDARDS	5
17.22.012 PROJECT SETTING	9
A. Setting and Location	9
B. Existing Site Conditions	9
C. Surrounding Land Uses and Development	9
17.22.014 ARCHITECTURAL AND SITE DESIGN GUIDELINES	10
A. Architectural Design Guidelines	10
1. Overall Project Theme	10
2. Massing and Scale	10
3. Articulation of Design	10
B. General Site Design Guidelines	13
1. Building Plotting Concepts	13
2. Vehicular Circulation	13
3. Pedestrian Circulation	13
4. Lighting	14
5. Water Quality Management Plan (WQMP)	17
6. Additional Site Design Parameters	17
a. Acoustical Buffers	17
b. Drive-Thrus/Screening	17
c. Trash Enclosures	18
d. Service, Loading, and Equipment Areas	18
e. Mechanical Equipment and Vents	23
17.22.016 LANDSCAPE DESIGN STANDARDS	23
A. Landscape Elements	23
B. Project Entry and Edge Conditions	27
1. Bedford Court Project Entry	27
2. South Edge/Existing Residential	27
C. Walls and Fences	27

List of Figures

Figure 1 Overall Site Plan	7
Figure 2 Conceptual Grading	8
Figure 3 Car Wash Elevation Examples	11
Figure 4 Coffee Shop Elevation Examples	12
Figure 5 Walkways and Pedestrian Linkages Plan	15
Figure 6 Lighting Examples	16
Figure 7 Trash Enclosures	21
Figure 8 Overall Landscape Plan	25
Figure 9 Plant Palette	26
Figure 10 Bedford Court/Project Entry	29
Figure 11a South Edge/Existing Residential Edge Condition (Car Wash)	30
Figure 11b South Edge/Existing Residential Edge Condition (Coffee Shop)	31
Figure 12 Wall and Fence Plan	32

List of Tables

Table 1 Schedule of Permitted Uses	2
Table 2 Development Standards Bedford Court PDO-16	5

17.22.002 TITLE

Section 17.22.002 through 17.22.016 shall be known as "PDO-16" (Bedford Court Planned Development Overlay District). (Ord. _____)

17.22.004 PURPOSE

The purpose of the Bedford Court Planned Development Overlay District ("PDO-16") is to permit creative mixtures of uses in smaller areas where a specific plan or the village center overlay is not appropriate. The planned development overlay zoning district is intended to provide a mechanism to allow for flexibility in the development regulations and design standards and to allow mixtures of uses and clustering of land uses that are traditionally prohibited by conventional zoning. Through this flexibility in standards, many of the objectives of the General Plan can best be achieved.

These requirements are intended to provide regulations for the development of primarily local-serving commercial uses, in a manner that is respectful to adjacent development. The Project will contain site-specific development standards and design guidelines which will include buffers and screening to the existing single-family residential units located to the south of the Project site.

17.22.006 RELATIONSHIP WITH THE MUNICIPAL CODE AND CITYWIDE DESIGN STANDARDS

- A. The permitted uses for the Bedford Court Planned Development Overlay District are described in Section 17.22.008.
- B. Except as modified by the provisions of Sections 17.22.008, 17.22.010, 17.22.012, or 17.22.016, the following rules and regulations shall apply to all planning applications in this area:
 - 1. The Citywide Design Guidelines that are in effect at the time an application is deemed complete.
 - 2. The approval requirements contained in the Municipal Code that are in effect at the time the application is deemed complete.
 - 3. Temecula Objective Design Standards for Multifamily and Mixed-Use Developments.
 - 4. Any other relevant rule, regulation, or standard that is in effect at the time an application is deemed complete. (Ord. 07-03 § 3)

17.22.008 USE REGULATIONS

PDO-16 is located at the terminus of Bedford Court, easterly of the I-15 freeway, and westerly of Temecula Parkway, and is intended to provide for the development of primarily local-serving commercial uses on approximately 1.88 gross acres.

PDO-16 is designed with common architectural massing and materials, a complementary color palette, and with consistent landscaping throughout the site, which will result in a comprehensive and cohesive development. PDO-16 will provide for a visually pleasing environment through adoption of supplemental performance standards that have been provided to ensure compatibility with the adjacent neighborhoods in terms of scale and

will be integrated into the existing development fabric by means of architectural theming, as well as buffering and setback requirements that go beyond those permitted by the underlying zoning designation, especially those pertaining to a car wash.

Table 1, *Schedule of Permitted Uses*, depicts the permitted and conditionally permitted uses that are allowed in PDO-16. Permitted uses are shown as a "P," conditionally permitted uses are shown as a "C."

Table 1 Schedule of Permitted Uses	
Description of Use	
A	
Aerobics/dance/gymnastics/martial arts studios (less than 5,000 sq. ft.)	P
Aerobics/dance/gymnastics/martial arts studios (greater than 5,000 sq. ft.)	P
Antique restoration	C
Antique sales	P
Apparel and accessory shops	P
Arcades (pinball and video games) ¹	C
Art supply stores	P
Automobile oil change/lube services with no major repairs	P
Automotive parts-sales	P
Automobile rental	C
Automobile repair services	C
Automobile sales (wholesale or auto broker only) with no outdoor/storage of vehicles	C
Automobile sales with only indoor display/storage of vehicles	C
Automobile service stations with or without an automated car wash ¹	P
Automotive service stations selling beer and/or wine- with or without an automated car wash ¹	C
B	
Bakery retail	P
Bank and financial institutions ¹	P
Barber and beauty shops	P
Beer and wine market ¹	C
Bicycle (sales, rental, services)	P
Billiard parlor/pool hall ^{1,2}	C
Blueprint and duplicating and copy services	P
Bookstores	P
Bowling alley ¹	P
Butcher shop	P
C	
Candy/confectionery sales	P
Car wash, full service ¹	C
Car wash, automated ¹	C
Community care facilities	P
Congregate care housing for the elderly ^{1,3}	P
Convenience market ¹	C
Cutlery	P

Table 1 Schedule of Permitted Uses	
Description of Use	
D	
Daycare centers	P
Delicatessen	P
Drug store/pharmacy	P
Dry cleaners	P
E	
Emergency shelters	C
Equipment sales and rentals (no outdoor storage)	P
F	
Fire and police stations	P
Floor covering sales	P
Florist	P
Fortunetelling, or similar activity	P
G	
Garden supplies and equipment sales and service	P
General merchandise/retail store (less than 10,000 sq. ft.)	P
Glass and mirrors, retail sales	P
Grocery store, retail ¹	P
H	
Health and exercise clubs (less than 5,000 sq. ft.)	P
Health care facility	P
Health food store	P
Hobby supply shop	P
I	
Ice cream parlor	P
Interior decorating service	P
L	
Laundromat	P
Libraries, museums, and galleries (private)	C
Locksmith	P
M	
Massage	P
Membership clubs, organizations, lodges	C
Medical equipment sales/rental	P
Musical and recording studio	C
N	
Nursing homes/convalescent homes	C
O	
Office equipment/supplies, sales/services	P
Offices, administrative, corporate headquarters and professional services including, but not limited to, business law, medical, dental, veterinarian, chiropractic, architectural, engineering, real estate and insurance	P
P	
Paint and wallpaper stores	P
Parcel delivery services	P
Parking lots and parking structures	C
Pawnshop	P
Personal service shops	P
Pet grooming/pet shop	P

Table 1 Schedule of Permitted Uses	
Description of Use	
Photographic studio	P
Postal services	P
Private utility facilities (regulated by Public Utilities Commission)	P
R	
Radio and broadcasting studios, offices	P
Recreational vehicle parks	C
Recreational vehicle sales and rentals	C
Religious institution, without a daycare or educational institution	P
Religious institution, with an educational institution	C
Religious institution, with a daycare center	C
Residential (one dwelling unit on the same parcel as a commercial or industrial use for use of the proprietor of the business)	C
Restaurant with drive-thru/fast food	C
Restaurant (bona fide public eating establishment) either without alcohol or with beer/wine only	P
Restaurant (bona fide public eating establishment) with distilled spirits ¹	C
Restaurant (bona fide public eating establishment) with entertainment (not including dancing) ^{1,2}	P
Restaurant (bona fide public eating establishment) with entertainment and/or dancing ^{1,2}	P
S	
Schools, trade or vocational	P
Senior citizen housing (see also congregate care) ³	P
Specialty market	P
Sports and recreation facility	C
T	
Taxi or limousine service	P
Tobacco shop	P
TV-VCR repair	P
W	
Watch repair	P
Wedding chapels	P
Wine tasting, with or without product sale for off-site consumption (Department of Alcoholic Beverage Control Type 02 only)	P
Wine tasting shop, with or without product sale for off-site consumption (Department of Alcoholic Beverage Control License Types other than Type 02)	C

¹Subject to the supplemental development standards contained in Chapter 17.09 of the City's Municipal Code.

²Subject to Chapter 9.10 (Entertainment License) and Chapter 9.11 (Regulation of Private Security Operators).

³Senior housing residential projects shall use the residential performance and development standards for the high-density residential zone. Congregate care facilities shall use the residential performance standards.

17.22.010 PDO-16 DESIGN STANDARDS

PDO-16 proposes one Planning Area, as depicted on Figure 1, *Overall Site Plan*. PDO-16 will be relatively close in grade to the existing Bedford Court roadway. Reference Figure 2, *Conceptual Grading*.

PDO-16 shall comply with the development standards set forth in Table 2, *Development Standards Bedford Court PDO-16*.

Table 2 Development Standards Bedford Court PDO-16	
LOT AREA	
Minimum gross area of site	0.50 acres
Target floor area ratio	.06
Floor area ratio range	.03 - .06
Maximum floor area ratio with intensity bonus as per Section 17.08.050 of the City's Municipal Code	1.0
Minimum front yard adjacent to a street	20'
Minimum yard adjacent to residentially zoned property	30'
Minimum interior side yard	0'
Minimum rear yard	10'
Minimum accessory structure - side/rear setback	5'
Minimum car wash to closest residential property line	75'*
Minimum building separation:	
One story	15'
Two story	20'
Three stories or more	25'
Maximum building height ¹	30' ²
Maximum wall height	6' ³
Maximum percent of lot coverage	10%
Minimum required landscaped open space ⁴	25%
¹ Height limits do not apply to masts, belfries, clock towers, chimney flues, elevator bulkheads, etc., provided they are located further than 50' from adjacent residences. ² Accessory structure height shall not exceed 20'. ³ Unless otherwise specified in any applicable noise study. ⁴ Minimum required landscaped open space area does not include any necessary planting within the right-of-way. * Project will incorporate design features that will ensure compliance with City's Noise Ordinance requirements for external and internal noise standards.	

PDO-16 Parking Requirements

The following parking requirements shall apply to PDO-16:

Parking requirements for PDO-16 shall be determined based on the specific use listed in Table 1, above, in accordance with City of Temecula Municipal Code Section 17.24.040, unless otherwise specified below.

The following requirements shall apply for drive-thru restaurants:

One (1) parking space/75 s.f. (gross) is required for drive-thru restaurants.
(*Note: patios are not counted in the parking calculation.*)

Required Americans with Disabilities Act (ADA) spaces, electric vehicle (EV) charging station spaces, and motorcycle stalls shall be provided.

The following requirements shall apply for car washes:

The maximum number of vehicles allowed in the wash tunnel x 2.5 is the requirement for a car wash.

Parking spaces shall include spaces for short-term parking associated with drying and vacuuming vehicles, required ADA drying and vacuuming spaces, and required EV charging station spaces.

Reciprocal access and parking shall be provided throughout PDO-16. A reciprocal access and parking agreement shall be approved by the City, and recorded against the property, prior to the recordation of a final map.



BEDFORD COURT

KEY NOTES	
1	EXISTING PROPERTY LINE
2	NEW STRENGTH
3	NEW PAINTED DISABLED SYMBOL
4	NEW DISABLED SIGN
5	NEW PLANTER
6	NEW OUTLET
7	NEW CURB
8	NEW SHORT TERM BIKE RACKS (4 BICYCLES)
9	NEW LONG TERM BIKE STORAGE
10	PROPOSED 2 BICYCLES (3 BICYCLES LOCATED)
11	NEW BIKE RACKS (4 BICYCLES)
12	NEW BIKE RACKS (4 BICYCLES)
13	NEW BIKE RACKS (4 BICYCLES)
14	NEW BIKE RACKS (4 BICYCLES)
15	NEW BIKE RACKS (4 BICYCLES)
16	NEW BIKE RACKS (4 BICYCLES)
17	NEW BIKE RACKS (4 BICYCLES)
18	NEW BIKE RACKS (4 BICYCLES)
19	NEW BIKE RACKS (4 BICYCLES)
20	NEW BIKE RACKS (4 BICYCLES)
21	NEW BIKE RACKS (4 BICYCLES)
22	NEW BIKE RACKS (4 BICYCLES)
23	NEW BIKE RACKS (4 BICYCLES)
24	NEW BIKE RACKS (4 BICYCLES)
25	NEW BIKE RACKS (4 BICYCLES)
26	NEW BIKE RACKS (4 BICYCLES)
27	NEW BIKE RACKS (4 BICYCLES)
28	NEW BIKE RACKS (4 BICYCLES)
29	NEW BIKE RACKS (4 BICYCLES)
30	NEW BIKE RACKS (4 BICYCLES)
31	NEW BIKE RACKS (4 BICYCLES)
32	NEW BIKE RACKS (4 BICYCLES)
33	NEW BIKE RACKS (4 BICYCLES)
34	NEW BIKE RACKS (4 BICYCLES)
35	NEW BIKE RACKS (4 BICYCLES)
36	NEW BIKE RACKS (4 BICYCLES)
37	NEW BIKE RACKS (4 BICYCLES)
38	NEW BIKE RACKS (4 BICYCLES)
39	NEW BIKE RACKS (4 BICYCLES)
40	NEW BIKE RACKS (4 BICYCLES)
41	NEW BIKE RACKS (4 BICYCLES)
42	NEW BIKE RACKS (4 BICYCLES)
43	NEW BIKE RACKS (4 BICYCLES)
44	NEW BIKE RACKS (4 BICYCLES)
45	NEW BIKE RACKS (4 BICYCLES)
46	NEW BIKE RACKS (4 BICYCLES)
47	NEW BIKE RACKS (4 BICYCLES)
48	NEW BIKE RACKS (4 BICYCLES)

FIGURE 1 - OVERALL SITE PLAN

Planned Development Overlay (PDO-16)

BEDFORD COURT

EXISTING COMMERCIAL
DEVELOPMENT
APN: 922-210-050
N.A.P.

EXISTING COMMERCIAL
DEVELOPMENT
APN: 922-210-045
N.A.P.

INTERSTATE 15 OFF RAMP

LEGEND

	PROPERTY LINE
	PROPOSED PROPERTY LINE
	CIVIL LIMITS OF WORK
	EASEMENT
	SLOPE
	STANDARD DUTY CONCRETE SIDEWALK
	HEAVY DUTY CONCRETE PAVEMENT
	STANDARD DUTY ASPHALT
	LANDSCAPE/PLANTER AREA
	DETECTABLE WARNING
	PROPOSED ELEVATION
	EXISTING ELEVATION

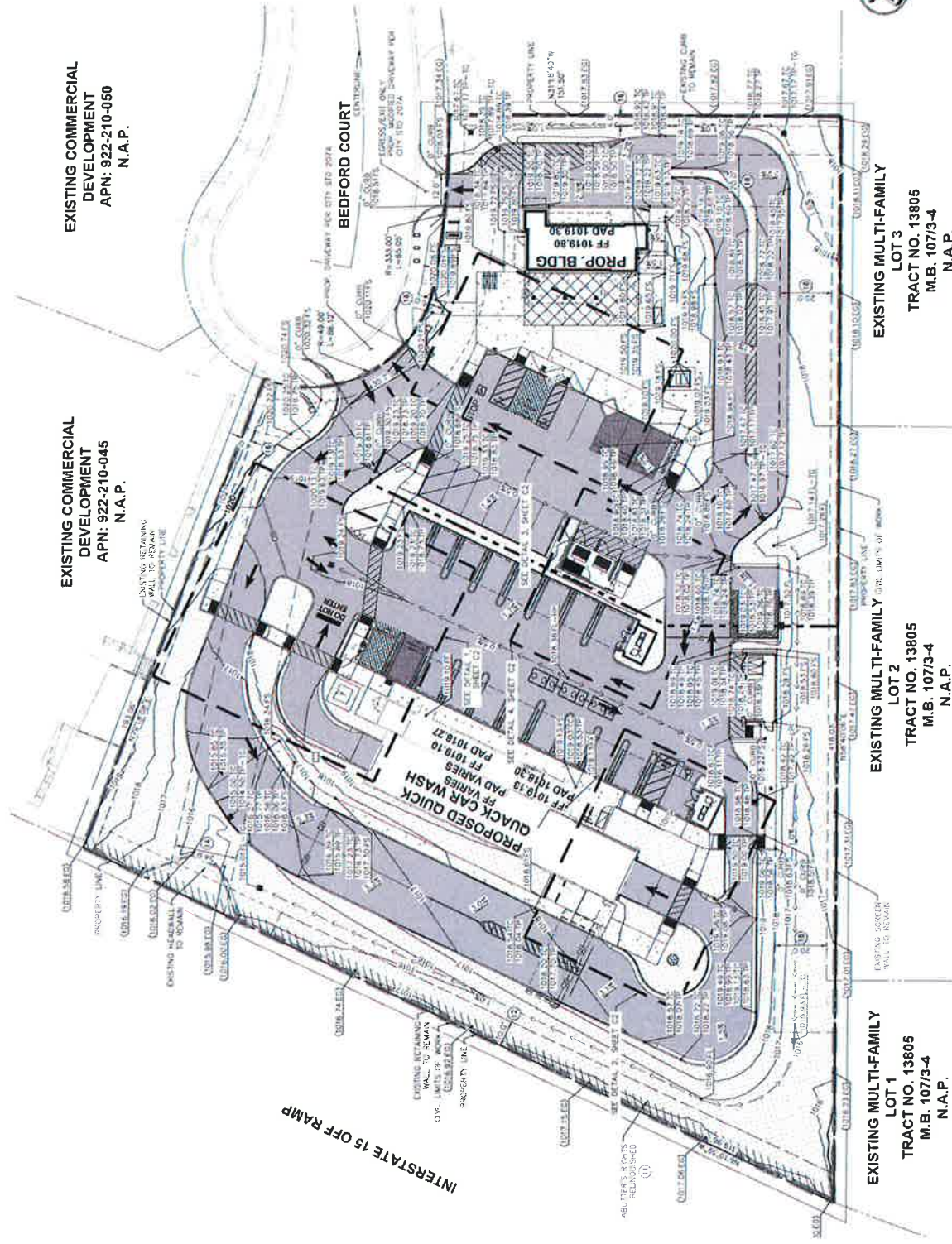


FIGURE 2 - CONCEPTUAL GRADING

17.22.012 PROJECT SETTING

A. Setting and Location

PDO-16 is comprised of approximately 1.88 gross acres located at the terminus of Bedford Court, easterly of the I-15 freeway, and westerly of Temecula Parkway.

B. Existing Site Conditions

The site is currently vacant and undeveloped. The project site is relatively flat with no areas of topographic relief. On-site elevation ranges from 1,014 to 1,025 feet above mean sea level. Soils on-site have been mechanically disturbed and heavily compacted from historic land uses (i.e., grading, routine weed abatement, illegal dumping, staging and stockpiling activities, and surrounding development). Drainage within the subject property generally flows to the west.

C. Surrounding Land Uses and Development

PDO-16 is bordered by High Density housing to the south, Highway Tourist Commercial to the immediate east and PDO-14 to the east across Temecula Parkway, Highway Tourist Commercial to the north, and I-15 to the west. The PDO-16 Project site is shown as the black bordered area in the aerial photo below.

PDO-16
Site and Surrounding Land Uses



Source: Map My County https://gis1.countyofriverside.us/Html5Viewer/index.html?viewer=MMC_Public

17.22.014 ARCHITECTURAL AND SITE DESIGN GUIDELINES

A. Architectural Design Guidelines

1. Overall Project Theme

PDO-16 has been designed by taking inspiration from a Contemporary Agrarian architectural style. This building style tends to focus on incorporating local materials into structures with farmhouse elements that are often rustic, organic, and comfortable, as conceptualized in Figure 3, *Car Wash Elevation Examples* and Figure 4, *Coffee Shop Elevation Examples*. The buildings are designed with common architectural massing and materials and a complementary color palette. The elements used reflect the City of Temecula Design Guideline requirements, while at the same time allowing for the ease of brand identification.

Additional details are included below. When all of these architectural guidelines are utilized together, the comprehensive essence of PDO-16 will be attained.

2. Massing and Scale

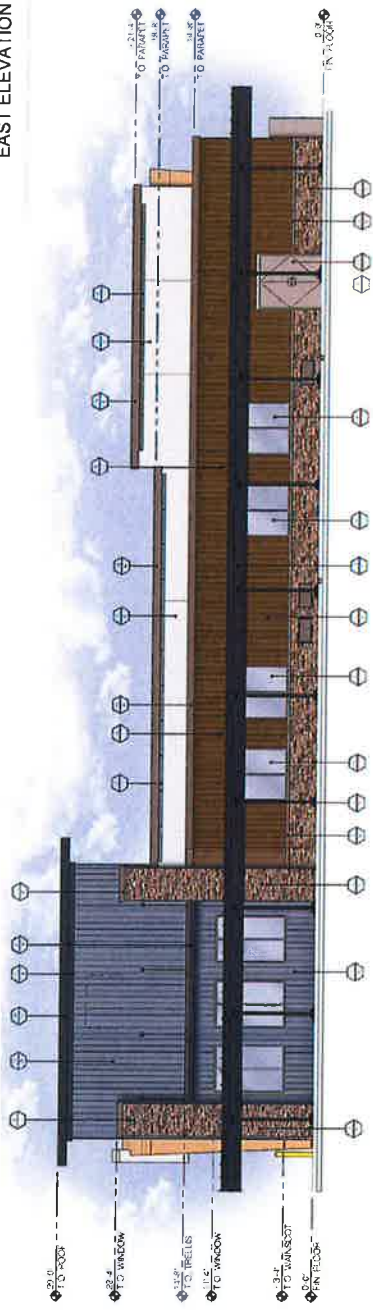
Building massing and scale should be sensitive to the existing built environment and shall be designed in a manner that compliments the existing development and the development within PDO-16. Buildings within PDO-16 may be up to 30' in height (as measured from finished pad elevation). Internal to the Project, buildings shall be sited in such a manner as to not impede, or seem to dominate, the adjacent buildings.

3. Articulation of Design

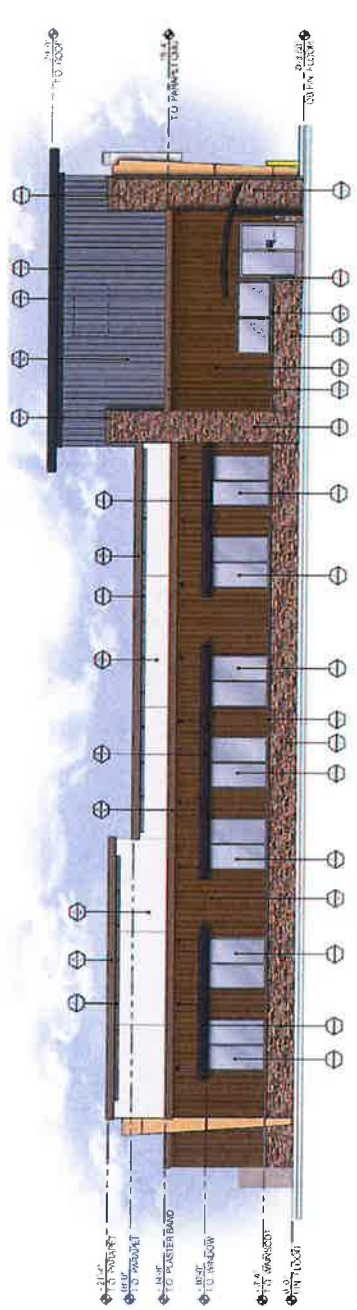
- The building design shall incorporate 360° architecture.
- Architectural elements that add interest and character to the prescribed architectural styles, such as recessed windows and shade canopies, shall be provided.
- High quality materials shall be used to create a look of permanence and a contemporary nature within PDO-16. Variations in color and material consistent with the standards established in Figure 3, *Car Wash Elevation Examples* and Figure 4, *Coffee Shop Elevation Examples* shall be utilized to create interest and reduce a monotonous appearance.
- Fixtures and finishes will be selected for their contribution to the overall theme of the development.

The building design shall incorporate 360° architecture. 360° architecture means the appropriate articulation of all building facades, including variation in massing, roof forms, and wall planes, as well as surface articulation to create shadow patterns and massing shapes that contribute to a building's character. It is expected that the highest level of articulation will occur on the front façade and facades visible from streets; however, similar and complementary massing, materials, and details are encouraged to be incorporated into the other building elevations. The use of elements such as overhangs, trellises, and awnings will be used to lend character to the building.

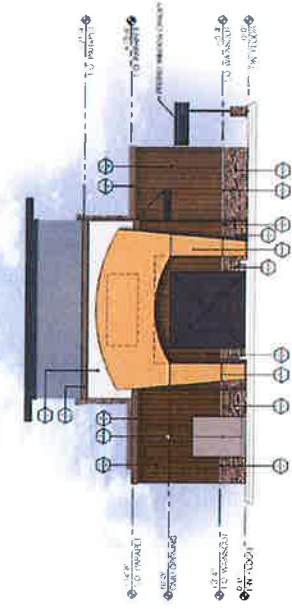
EAST ELEVATION



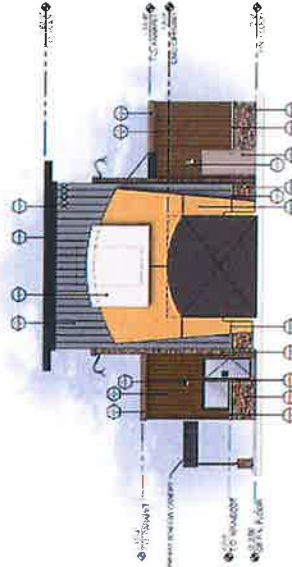
WEST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION



BEDFORD COURT

MATERIALS	
1	CONCRETE PLASTER SAND FINISH
2	WHITE WALL GROUP SYSTEM
3	STONE MOUNTAIN
4	BRICKWORK (SANDWICH) BLACK GRAYW
5	PER TERRAZZO SANDWICH GLASS
6	WOODGRAIN LIGHT HONEY
7	ALUMINUM GLAZING
8	WOOD GRAY
9	ALUMINUM SCHED ROW SYSTEM
10	BRICKWORK (SANDWICH) BLACK GRAYW
11	ALUMINUM PLASTER FINISH
12	WHITE FLOOR
13	WHITE BOARD
14	WHITE ROOF
15	WOOD GRAY
16	WHITE BOARD
17	WHITE BOARD
FINISHES	
1	CONCRETE PLASTER - WHITE WASH
2	CONCRETE PLASTER - LIGHT GRAY
3	CONCRETE PLASTER - SANDWICH
4	CONCRETE PLASTER - SANDWICH
5	CONCRETE PLASTER - SANDWICH
6	CONCRETE PLASTER - SANDWICH
7	CONCRETE PLASTER - SANDWICH
8	CONCRETE PLASTER - SANDWICH
9	CONCRETE PLASTER - SANDWICH
10	CONCRETE PLASTER - SANDWICH
11	CONCRETE PLASTER - SANDWICH
12	CONCRETE PLASTER - SANDWICH
13	CONCRETE PLASTER - SANDWICH
14	CONCRETE PLASTER - SANDWICH
15	CONCRETE PLASTER - SANDWICH
16	CONCRETE PLASTER - SANDWICH
17	CONCRETE PLASTER - SANDWICH

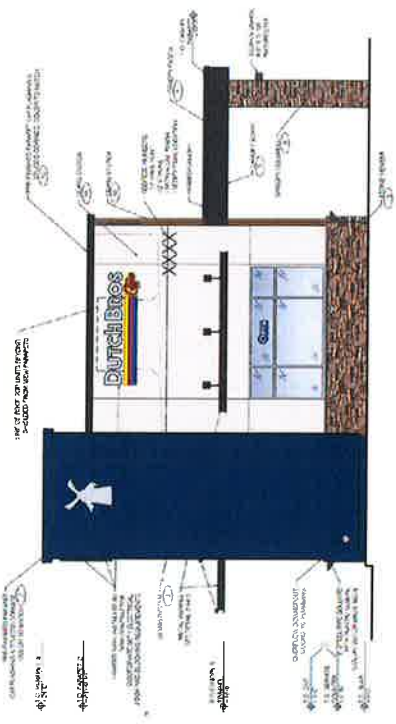
FIGURE 3 - CAR WASH ELEVATION EXAMPLES

BEDFORD COURT

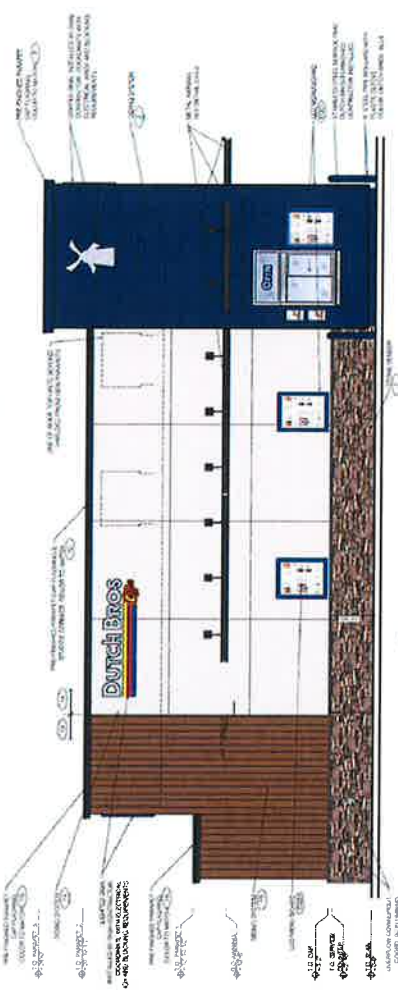
WALK UP WINDOW ELEVATION WEST



FRONT ELEVATION NORTH



DRIVE THROUGH WINDOW ELEVATION EAST



REAR ELEVATION SOUTH



FIGURE 4 - COFFEE SHOP ELEVATION EXAMPLES

B. General Site Design Guidelines

These general site design guidelines are not intended to restrict taste or preference but are designed to foster harmony of development. Various site design elements elements/concepts shall be considered to create a high-quality commercial development.

In order to encourage a higher level of design direction and quality, the site design elements have been included in PDO-16 and are discussed in greater detail below:

- Building Plotting Concepts
- Streetscapes
- Vehicular Circulation
- Pedestrian Circulation
- Lighting
- Water Quality Management Plan (WQMP)

Reference Figure 1, *Overall Site Plan*, as well as the Figures in Section 17.22.016 (Landscape Design Standards) of PDO-16.

1. Building Plotting Concepts

Building masses should be articulated to avoid long straight building facades and create enclosed patios and pedestrian spaces where possible. The plotting of buildings shall take into account shade elements and maximize shade created by surrounding buildings.

2. Vehicular Circulation

Vehicles enter the Project site via Bedford Court and may travel throughout the site in two directions via a loop system. Directional signage for drive-thrus shall be provided. Uses within the site shall take access off of the Project circulation loop/roadway. Circulation throughout the site shall comply with applicable Building and Fire codes and stacking shall comply with any queueing analyses prepared for the uses.

Focal elements, which provide a sense of arrival, are incorporated into the design of the Project entry. These elements include but are not limited to enhanced landscaping and monumentation. Incorporation of these elements will ensure a sense of vehicular arrival to PDO-16. Landscaping shall be incorporated to soften the impact of paved areas. This will be accomplished through the use of elements such as, but not limited to, shading, accents, and other green elements.

- Low freestanding walls incorporating simple, tasteful signage with pilasters, balustrades, finials and other embellishments may be proposed.
- All driveway dimensions, with the exception of those adjacent to any mid-rise structures, shall be a minimum of 24' wide. Dimensions for driveways adjacent to mid-rise structures shall comply with Building Code requirements.

3. Pedestrian Circulation

A pedestrian circulation system, interconnected by ADA compliant walkways and sidewalks, shall be provided consistent with Figure 5, Walkways and Pedestrian Linkages Plan. Decorative hardscapes shall be provided at the Project entry and at pedestrian crossings consistent with Figure 8, Overall Landscape Plan.

4. Lighting

Exterior lighting can provide safe and effective evening character illumination for the pedestrian and vehicular areas of drive aisles, sidewalks, and walkways through PDO-16. Design should reflect the concept and character of PDO-16 through illumination level, light fixture type, finish color and location.

Residential Interface

Special attention shall be paid to the interface of PDO-16 and the existing residences to the south. A variety of lighting methods shall be employed in order to retain the existing nighttime views of these residents. This can be accomplished through the following methods:

- Use low level bollards on-site for portions of the Project site that are adjacent to the residences.
- Fully shield all lights that are taller than bollards.

On-site lighting shall comply with the following:

- The level of on-site lighting shall comply with any and all applicable requirements of the City of Temecula and the Mount Palomar Lighting Ordinance.
- All exterior lighting fixtures shall be consistent with the architectural style for the building that it serves.
- Lighting shall be low-voltage/high-efficiency whenever possible.
- Lights shall be focused downward to ensure pedestrian safety and way finding.
- Footpath lights are acceptable as a means to illuminate a secondary path.
- Inset stairway and stair step lights are encouraged to ensure pedestrian safety and way-finding.
- Building mounted lighting fixtures shall be selected and located to cast downward and be shielded to minimize glare.
- Accent lighting shall emphasize special features such as fountains (only allowed with reclaimed or recycled water), sculptures, wall niches, signs, planters or accent trees for decorative effects.
- Accent lighting shall be inconspicuous and durable.
- Small scale accent lights such as LED based fixtures can serve for way-finding or be used themselves as special design elements.
- Flashing, neon, moving, high-intensity or exposed light source type luminaries are not permitted.

Refer to Figure 6, *Lighting Examples*

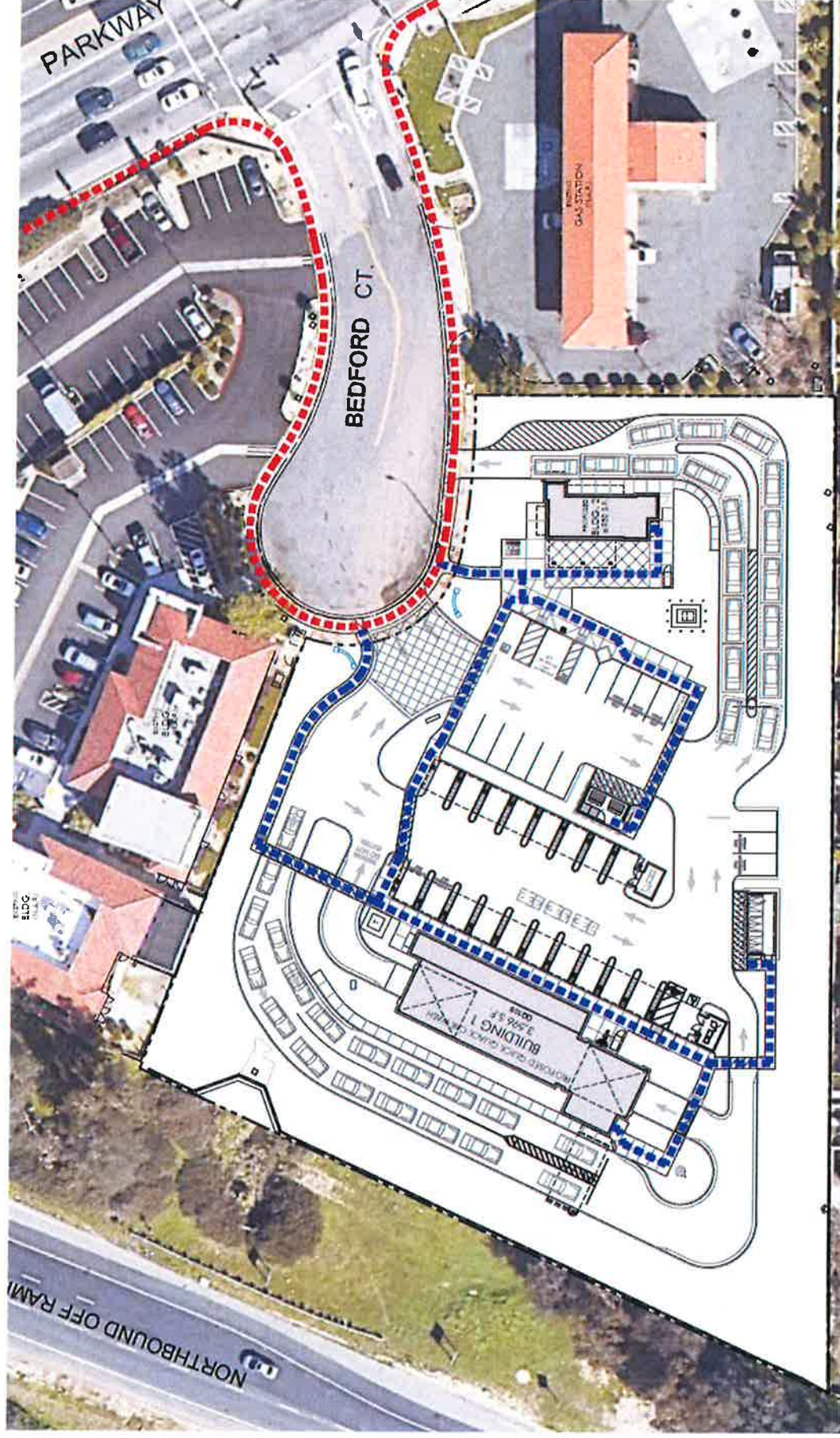


FIGURE 5 - WALKWAYS AND PEDESTRIAN LINKAGES PLAN

Planned Development Overlay (PDO-16)



CANOPY LIGHTING:

LINEAR LIGHTING:
MODEL: SLED LINEAR LED
FACADE FIXTURE
TYPE: SLED2/F1 (2-FOOT)
COLOR: PLATINUM SILVER
DISTRIBUTION: DOWNLIGHT
ONLY
BY: SECURITY LIGHTING



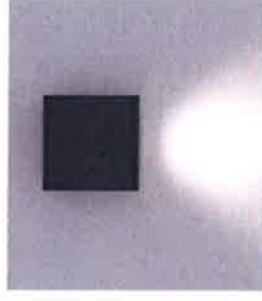
WALL MOUNT FIXTURE
SERIES SIZE: WS1611GV/
WALL MOUNTING: PA24
SHADE DIMENSION: 16" DIA.
FINISH COLOR: BRUSHED ALUMINUM
MANUFACTURER: RLM CLASSICS



AREA LIGHT:
MODEL: GLEON GALLEON
COLOR: BRONZE
BY: MCGRAW-EDISON



26 WATT LED WALLPACK SECURITY LIGHT
WITH EMERGENCY BACKUP BATTERY
MODEL: WPLED26Y/E
COLOR: BRONZE
BY: IMPERIAL LIGHTING



MATRIX 4 SURFACE
WALL MOUNTED COMPACT CUBIC
MODEL: UMT-31426-DBNA/E
COLOR: MATTE SILVER RAL 9006
BY: LIGMAN LIGHTING

NOTE: SIMILAR BRANDS/MODELS MAY BE USED PROVIDED THEY HAVE SIMILAR QUALITY AND MEET THE DESIGN INTENT

5. Water Quality Management Plan (WQMP)

Ultimate design of PDO-16 will require compliance with City and State water quality requirements. Each implementing Project (i.e., Development Plan, Subdivision Map, or Conditional Use Permit) will require the preparation of a Water Quality Management Plan (WQMP). While it is known that a WQMP will be required, the exact nature and location of the WQMP components are not identified at this time. The following is a list of Best Management Practices (BMPs) that may be required for the subsequent development within PDO-16:

- Filtration BMP.
- Harvest & Use.
- Self-Treated Areas.
- Bio Retention Planters.

6. Additional Site Design Parameters

The following are additional site design considerations that will be required within PDO-16:

a. Acoustical Buffers

Acoustical buffers will be required for any car wash use. In order to reduce any noise impacts from these uses to the adjacent residences to the south, the following design features shall be required:

- All blowers associated with drying vehicles and vacuums used for cleaning interiors of vehicles shall be oriented away from residences to the south.
- All blowers shall be internalized.
- Blowers shall utilize a muffler/silencer, shall meet or exceed current industry standards for noise reduction, and shall be implemented as required by any applicable noise study.
- Absorptive material shall line the last 15' of the tunnel exit on the walls and ceiling and shall be implemented as required by any applicable noise study.
- All noise emanating from the blowers and vacuums shall be lower than the City's threshold for noise at the property line for any residential sensitive receptor.

b. Drive-Thrus

Noise and light impacts from drive-thru operations within PDO-16 will be addressed through design and strategic siting of the noise and light generating activities. The existing screen wall, as currently constructed, is sufficient to prevent vehicle lights from impacting residences to the south. If any new walls are proposed in the future, they must also sufficiently screen vehicle lights. The following design features shall be required:

- Drive-thrus shall be screened from the public right-of-way and from the residences to the south. If utilizing a speaker box, it shall be oriented away from the existing residences.
- Landscape screening shall be provided along the property edges in order to provide additional means of reducing any potential noise and lighting impacts.

- A pickup window canopy structure shall be provided. Canopies must be architecturally integrated and shall utilize similar materials as the other shade structures on site.

Reference Figure 11a, *South Edge/Existing Residential Edge Condition (Car Wash)* and Figure 11b, *South Edge/Existing Residential Edge Condition (Coffee Shop)* provided in Section 17.22.016 (Landscape Design Standards) of PDO-16.

c. Trash Enclosures

The following design features, as depicted on Figure 7, *Trash Enclosure*, shall be required:

- Trash enclosures should be separated from adjacent parking stalls with a minimum of 5' wide (interior clear dimension) planter and a 12" wide paved surface behind the curb to ensure adequate space is available for individuals to access a vehicle.
- Trash/recycling containers should be large enough, placed frequently throughout the site, and collected frequently enough to handle the refuse generated by the Project.
- Trash enclosures shall be designed with similar finishes, materials, and details as depicted on Figure 3, *Car Wash Elevation Examples*, and shall be screened with vines and landscaping.
- Chain link fencing and gates with wood slats are not allowed.
- Enclosures should be unobtrusive and conveniently located for trash disposal by tenants and for collection service vehicles.
- A pedestrian entrance to the trash enclosure shall be provided so that large access doors do not have to be opened.
- Enclosures shall not be visible from primary entry drives.
- Enclosures shall have a concrete apron onto which trash/recycling containers will be rolled for collection.
- All trash enclosures shall comply with the City of Temecula requirements.

d. Service, Loading, and Equipment Areas

The following Guidelines shall be implemented within PDO-16 as they pertain to service, loading and equipment areas in order to ensure that these functional site items are integrated with a sense and respect of the surrounding aesthetic:

1. Storage for supplies, merchandise, and similar materials shall be prohibited on the roofs of any buildings.
2. Merchandise, material, and equipment are not permitted to be stored to a height greater than any adjacent wall, fence, or building.
3. Service, loading, and storage areas shall be separated from pedestrian and private automobile circulation.
4. Service areas, loading docks, and equipment areas shall be screened from views either by locating these uses within a building or by screening them with landscaping, walls, fences, or other architectural treatments.
5. Any outdoor display ancillary to the business being conducted within the building may be permitted without a temporary use permit, provided that the display

Bedford Court

complies with the regulations contained within Section 17.10.020 K. of the Supplemental Development Standards of the City's Municipal Code.

This page left intentionally blank for pagination purposes.

BEDFORD COURT

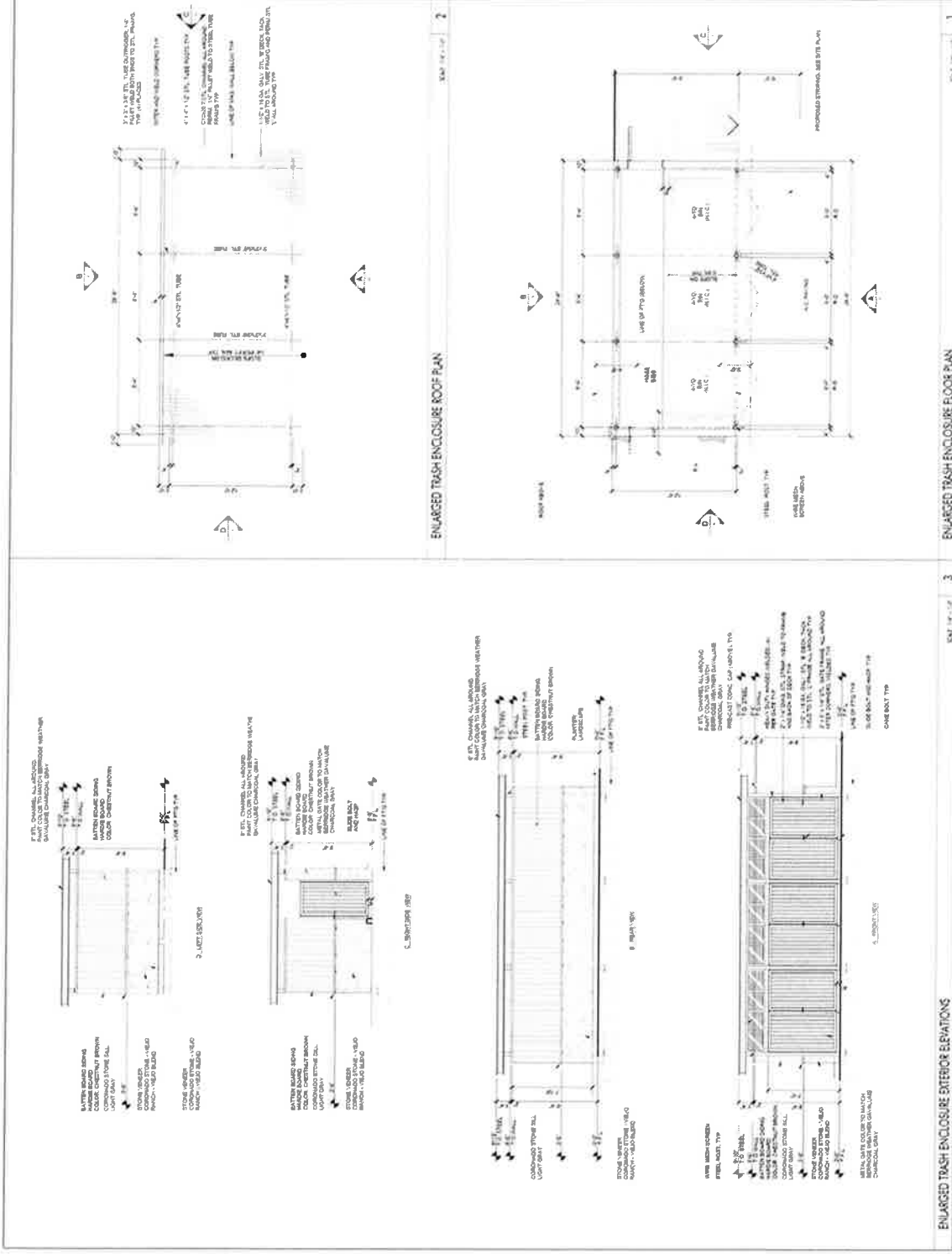


FIGURE 7 - TRASH ENCLOSURES

BEDFORD COURT

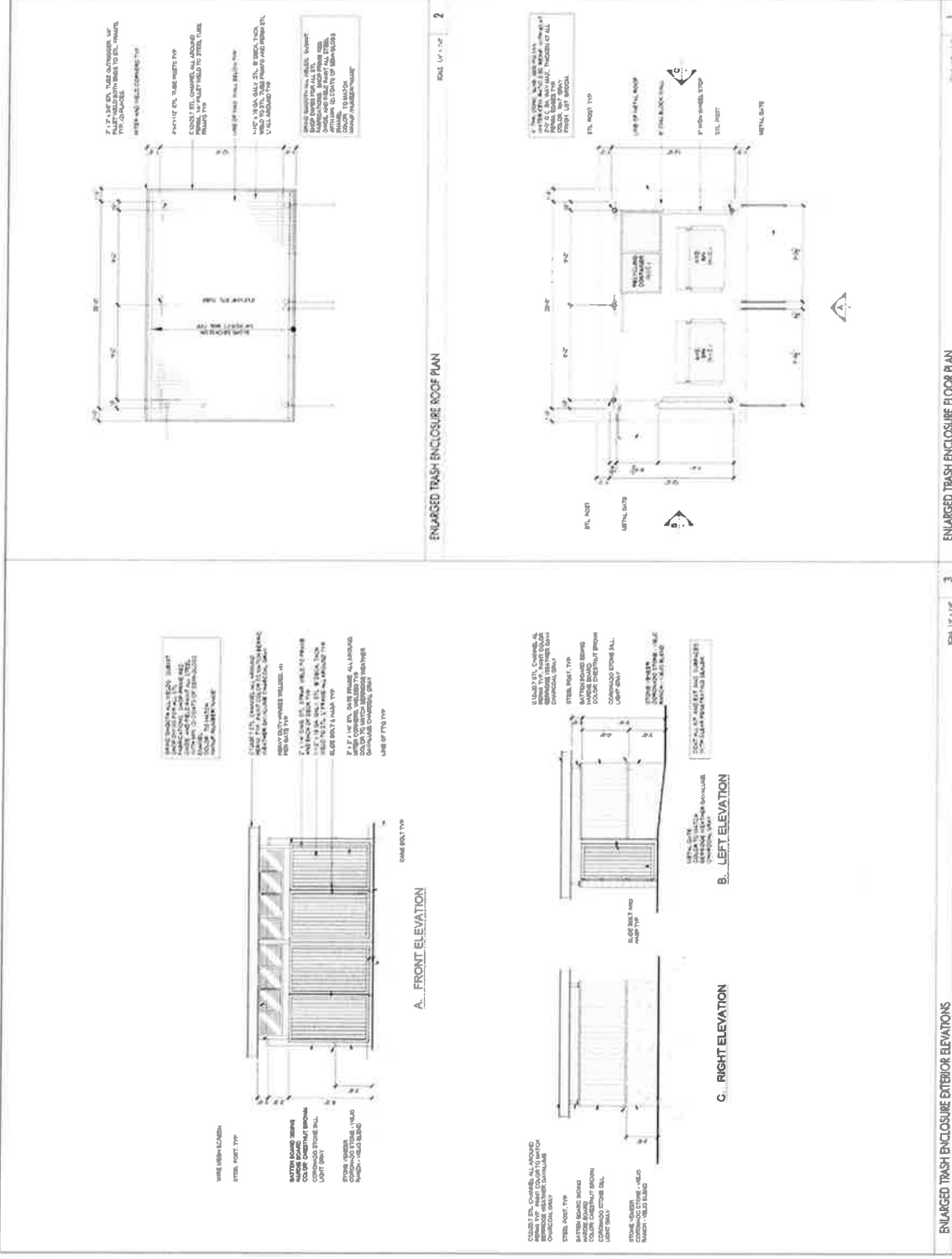


FIGURE 7 - TRASH ENCLOSURES, CONT.

e. Mechanical Equipment & Vents

- On-site mechanical equipment visible from buildings or public streets shall be screened.
- Wall mounted mechanical equipment that protrudes more than six inches from the outer building wall shall be screened from view by structural features that are compatible with the architecture or the subject buildings. Wall mounted mechanical equipment that protrudes six inches or less from the outer building wall shall be designed to blend with the color and architectural design of the subject building.
- Ground mounted mechanical equipment shall be screened from view by a decorative architectural structure or landscape screening that is compatible with the architecture and landscaping of the development site. Such screening devices shall be of a height equal to or greater than the height of the mechanical equipment being screened.
- Roof flashing vents exposed to public view shall be painted or otherwise given a finish to match adjacent surfaces or concealed in a manner consistent with the building's appearance.

17.22.016 LANDSCAPE DESIGN STANDARDS

A. Landscape Elements

The landscape design of PDO-16 shall conform to Section 17.08.060 (Commercial/Office/Industrial Districts, Landscape Standards) of the City's Municipal Code, unless otherwise specified below. Figure 8, *Overall Landscape Plan*, and Figure 9, *Plant Palette*, have been developed to provide the guidance for the landscape theme for PDO-16.

Landscape plans shall be required as a part of any Development Plan or Conditional Use Permit for PDO-16 and shall include the following:

- A combination of ornamental plants and the use of drought-tolerant plant materials.
- Streetscape planted with a minimum 24" box specimen size street tree spaced at 30' on center spacing (maximum) with a combination of 5-gallon and 1-gallon shrubs and ground cover.
- Common landscaped with appropriate shrubs, trees and ground cover and maintained by a Master Property Owners Association for PDO-16.

The following shall also be required as a part of any Development Plan or Conditional Use Permit for PDO-16:

- Sidewalks at building entries only shall be a minimum of 11' wide where adjacent to head in parking to allow for car bumper overhang. Additionally, a minimum landscape strip of 6' is required and shall be provided between a building and parking where there is no pedestrian space.
- One landscaped finger island shall be provided per every 10 spaces. Landscape islands shall be a minimum of 5 feet (inside dimension) in width to allow for tree growth and to avoid tree trunks from being hit. These shall also include a 1'-6" combination curb/step out.

- Exceptions to this requirement shall be allowed underneath car wash vacuum canopies and adjacent to required ADA paths of travel.
- Trees and shrubs shall be placed a minimum of 5' away from water meter, gas meter, or sewer laterals; a minimum of 10' away from utility poles; and a minimum of 8' away from fire hydrants and fire department sprinkler and standpipe connections.
- Drip irrigation is required wherever possible. Overhead spray irrigation is not allowed within 24" of any non-permeable surface.



Planned Development Overlay (PDO-16)

FIGURE 8 - OVERALL LANDSCAPE PLAN
25

BEDFORD COURT

Symbol	Botanical Name	Common Name	WUCOLS REGION 4
--------	----------------	-------------	-----------------

	<i>Alnus marina</i>	Strawberry Tree	Mod
	<i>Cercis occidentalis</i>	Western Redbud	Low
	<i>Chilopsis l. 'Bubba'</i>	Desert Willow	Low
	<i>Geijera parviflora</i>	Australian Willow	Low
	<i>Ginkgo biloba 'Autumn Gold'</i>	Maidenhair Tree	Low
	<i>Pinus canariensis</i>	Canary Island Pine	Low
	<i>Pistacia chinensis</i>	Chinese Pistache	Mod
	<i>Prosopis chilensis</i>	Chilean Mesquite	Low
	<i>Podocarpus gracillior</i>	Fern Pine	Low
	<i>Washingtonia filifera</i>	California Fan Palm	Low

VINES	<i>Lonicera japonica 'Halliana'</i>	Hall's Honeysuckle	Low
-------	-------------------------------------	--------------------	-----

SCREEN SHRUBS	<i>Elaeagnus pungens</i> *	Silverberry	Low
	<i>Leucophyllum frutescens</i> *	Texas Ranger	Low
	<i>Rhamnus californica 'Eve Case'</i> *	Dwarf Cateberry	Low
	<i>Westringia 'Blue Gem'</i> *	Blue Gem Coast Rosemary	Low

* Identifies as selected from the Uptown Jefferson Specific Plan - Approved Plant List

Symbol	Botanical Name	Common Name	WUCOLS
--------	----------------	-------------	--------

SHRUBS/ GROUNDCOVERS

	<i>Achillea millefolium</i>	Common Yarrow	Low
	<i>Bouteloua g. 'Blonde Ambition'</i>	Blue Grama Grass	Low
	<i>Callistemon v. 'Little John'</i>	Little John Bottlebrush	Low
	<i>Cistus purpureus</i>	Orchid Rock Rose	Low
	<i>Euphorbia rigida</i>	Gopher Plant	Low
	<i>Festuca mairei</i>	Atlas Fescue	Mod
	<i>Helictotriton sempervirens</i>	Blue Oat Grass	Mod
	<i>Hesperaloe parviflora</i>	Red Yucca	Low
	<i>Lantana 'New Gold'</i>	New Gold Lantana	Low
	<i>Lavandula 'Meerlo'</i>	Meerlo English Lavender	Low
	<i>Leucophyllum f. 'Compacta'</i>	Texas Ranger	Low
	<i>Ligustrum japonica 'Texanum'</i>	Texas Privet	Mod
	<i>Lonicera j. 'Halliana'</i>	Halls Honey Suckle	Low
	<i>Macfadyena unguis-cati</i>	Cat's Claw	Low
	<i>Muhlenbergia capitaris</i>	Pink Muhly	Low
	<i>Myoporum parvifolium</i>	Myoporum	Low
	<i>Myrtus communis 'Compacta'</i>	Dwarf Myrtle	Mod
	<i>Phormium tenax</i>	Hybrid New Zealand Flax	Mod
	<i>Pennisetum a. 'Bunny Tails'</i>	Little Bunny Fountain Grass	Low
	<i>Penstemon h. 'Margarita BOP'</i>	Foothill Penstemon	Low
	<i>Rosemarinus o. 'Huntington Carpet'</i>	Prostrate Rosemary	Low
	<i>Salvia greggii</i>	Autumn Sage	Low
	<i>Senecio mandraliscae</i>	Blue Chalksticks	Low

LANDSCAPE NOTES

PLANT MATERIAL NOT LISTED MAY BE USED, SUBJECT TO APPROVAL BY THE CITY.

ALL LANDSCAPE PLANS AND INSTALLATIONS SHALL ADHERE TO CITY DESIGN GUIDELINES, CODES AND REGULATIONS.

ALL LANDSCAPE AREAS SHALL RECEIVE AUTOMATIC IRRIGATION SYSTEM.

COMMON LANDSCAPE INSTALLATION SHALL BE PERMANENTLY MAINTAINED BY PROPERTY OWNERS ASSOCIATION.

ALL TREES TO BE 10' CLR. OF FIRE HYDRANTS, 8' CLR. OF WATER METERS AND 20' CLR. OF LIGHT STANDARDS

ALL SHRUBS TO HAVE A MINIMUM CONTAINER SIZE OF 5 GALLONS.

FIGURE 9 - PLANT PALETTE

26

B. Project Entry and Edge Conditions

The following text and figures describe the streetscapes contained in PDO-16. The streetscapes have been designed in a comprehensive manner to provide continuity within PDO-16 as well as compatibility with the existing, surrounding development.

1. Bedford Court Project Entry

The Project entry is located on Bedford Court. The design of this entry is depicted on Figure 10, *Bedford Court Project Entry*. Utilizing the Plant Palette (Figure 9), both sides of the entry will include enhanced landscaping, which includes trees and shrubs.

2. Southern Edge/Existing Residential

The southern edge of the Project site is depicted in Figure 11a, *South Edge/Existing Residential Edge Condition (Car Wash)* and Figure 11b, *South Edge/Existing Residential Edge Condition (Coffee Shop)*. Utilizing the Plant Palette (Figure 9), this edge of the site will be planted with an array of trees, shrubs, dense and groundcovers, along with earth berms adjacent to the car wash for additional noise mitigation.

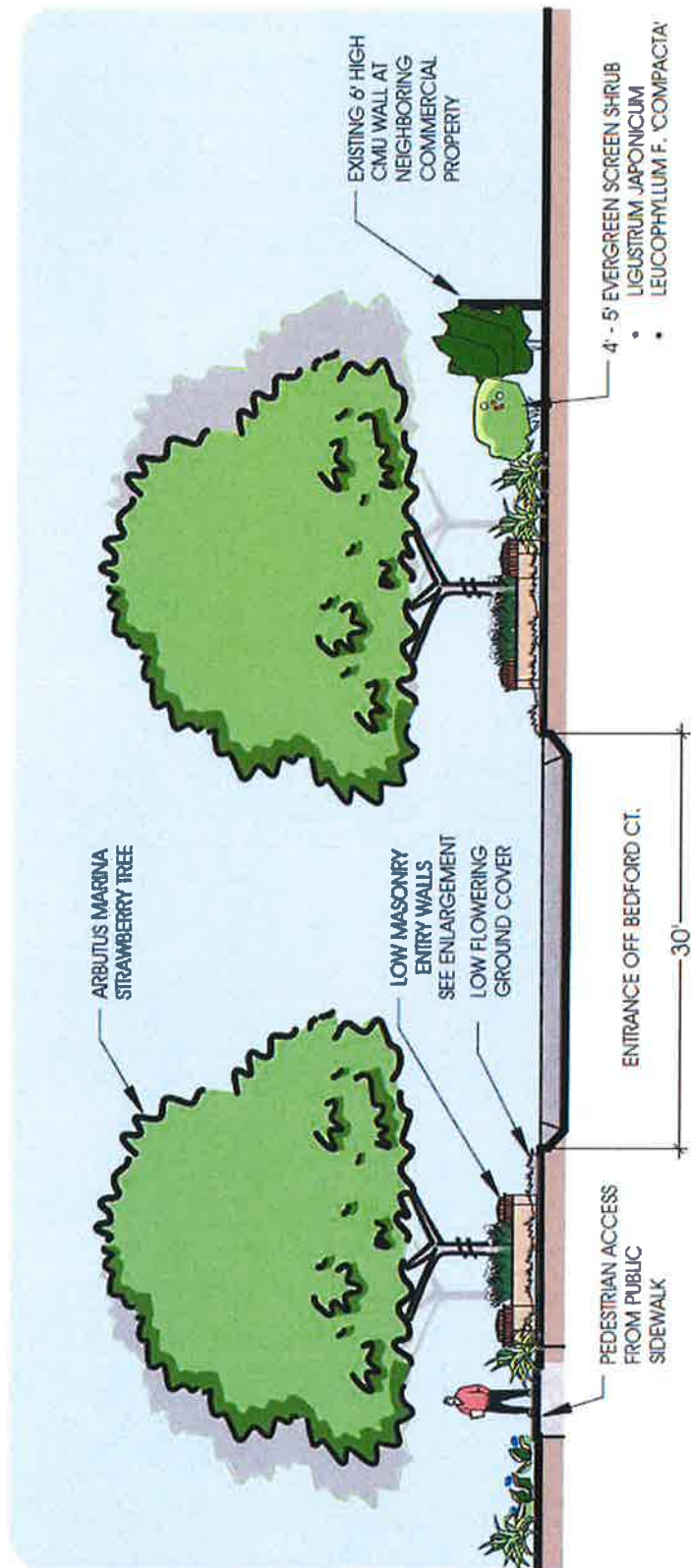
C. Walls and Fences

Walls provide edges, grade retention, spatial definition, and privacy. Walls and fences are particularly critical along the Project's southern edge, as it is the most sensitive edge. The following guidelines shall apply to PDO-16:

- Landscaping should be used to soften walls.
- Landscape materials will complement the architectural theme.
- Stone veneer, masonry, block and wrought iron combinations are acceptable.
- Wood fence material should be of the quality to stain so as to prevent rotting and weathering.
- Walls may be covered with stucco in colors suitable to the architectural theme.
- Stone surfaces may remain natural and unpainted.
- Materials, colors and textures shall be varied to create interest and relieve visual monotony.
- Barbed wire, wire, electrically charged fences, corrugated metal, chain link, and grape-stake fencing is prohibited.

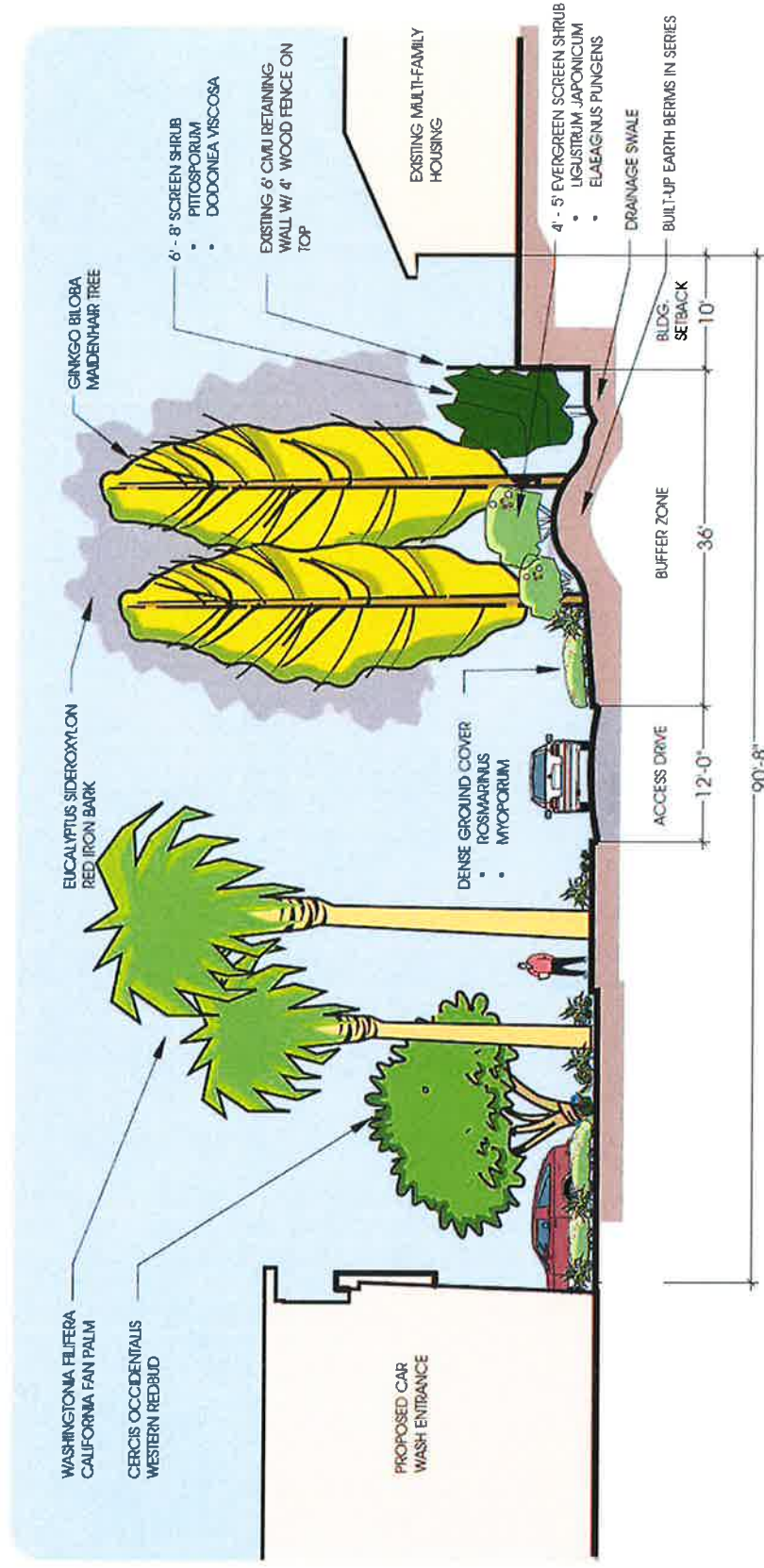
Reference Figure 12, *Wall and Fence Plan*.

This page left intentionally blank for pagination purposes.



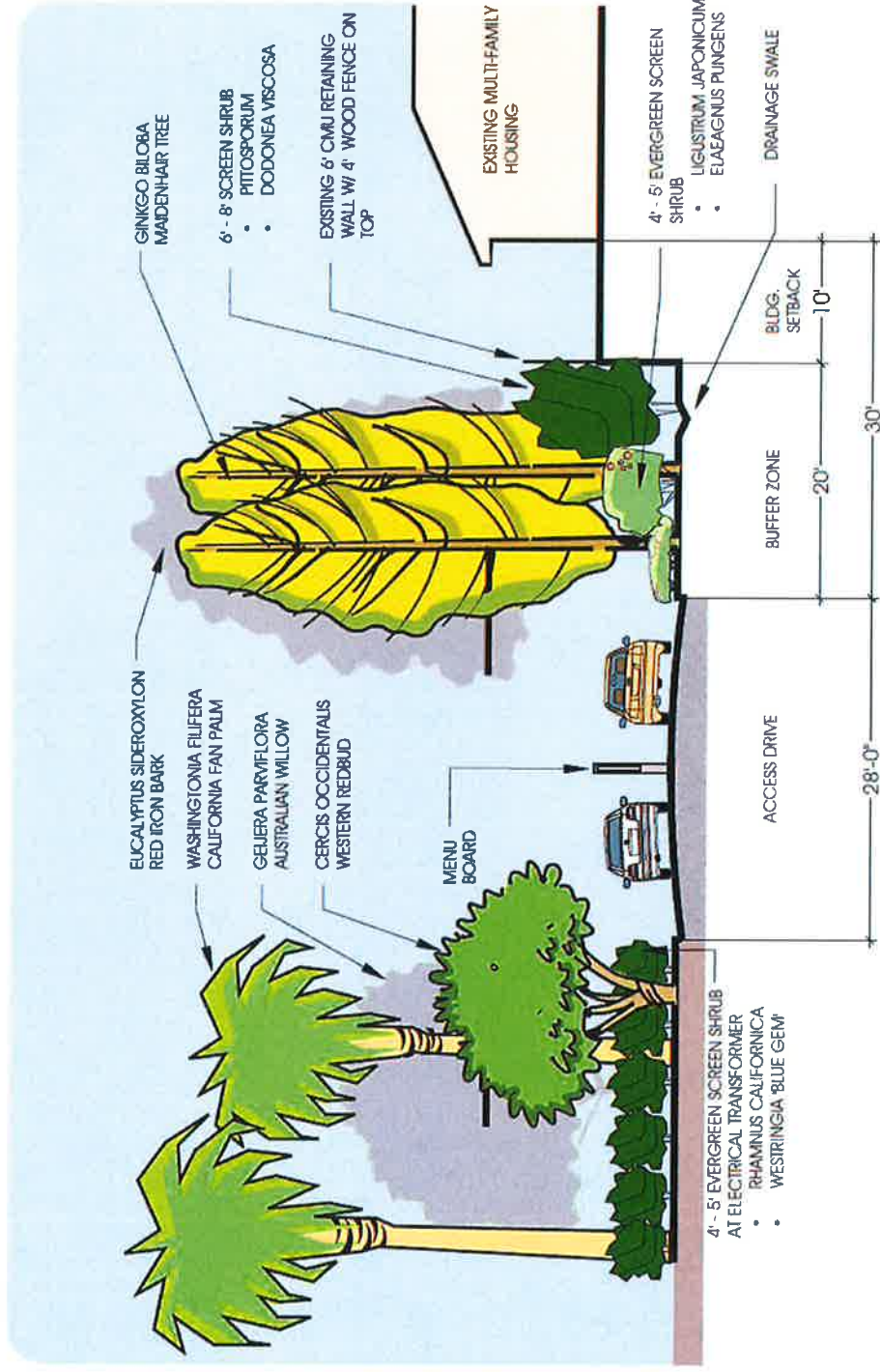
SECTION A, ENTRY - REFER TO FIGURE 8, LANDSCAPE PLAN
MINIMUM RIGHT-OF-WAY SETBACK @ 5 YEARS

FIGURE 10 - BEDFORD COURT/
PROJECT ENTRY



SECTION B, CAR WASH ACCESS - REFER TO FIGURE 8, LANDSCAPE PLAN
VEGETATION BUFFER @ 5 YEARS

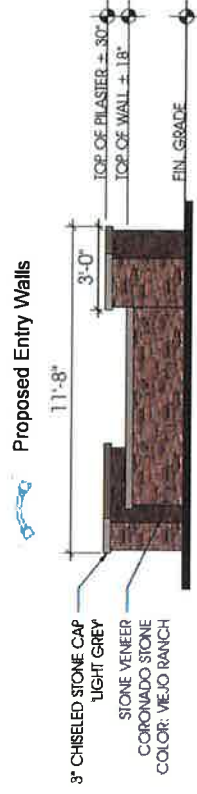
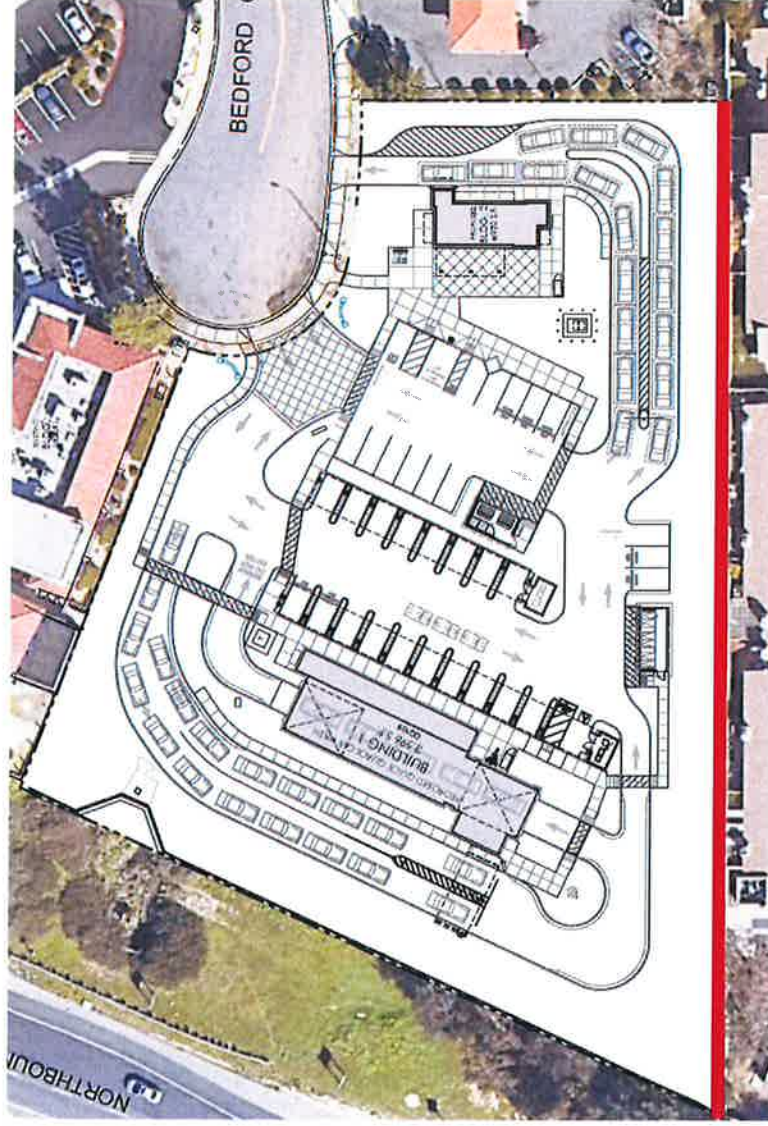
FIGURE 11A - SOUTH EDGE/EXISTING RESIDENTIAL
EDGE CONDITION (CAR WASH)



SECTION C, DRIVE-THRU COFFEE ACCESS - REFER TO FIGURE 8, LANDSCAPE PLAN
VEGETATION BUFFER @ 5 YEARS

FIGURE 11B - SOUTH EDGE/EXISTING RESIDENTIAL
EDGE CONDITION (COFFEE SHOP)

BEDFORD COURT



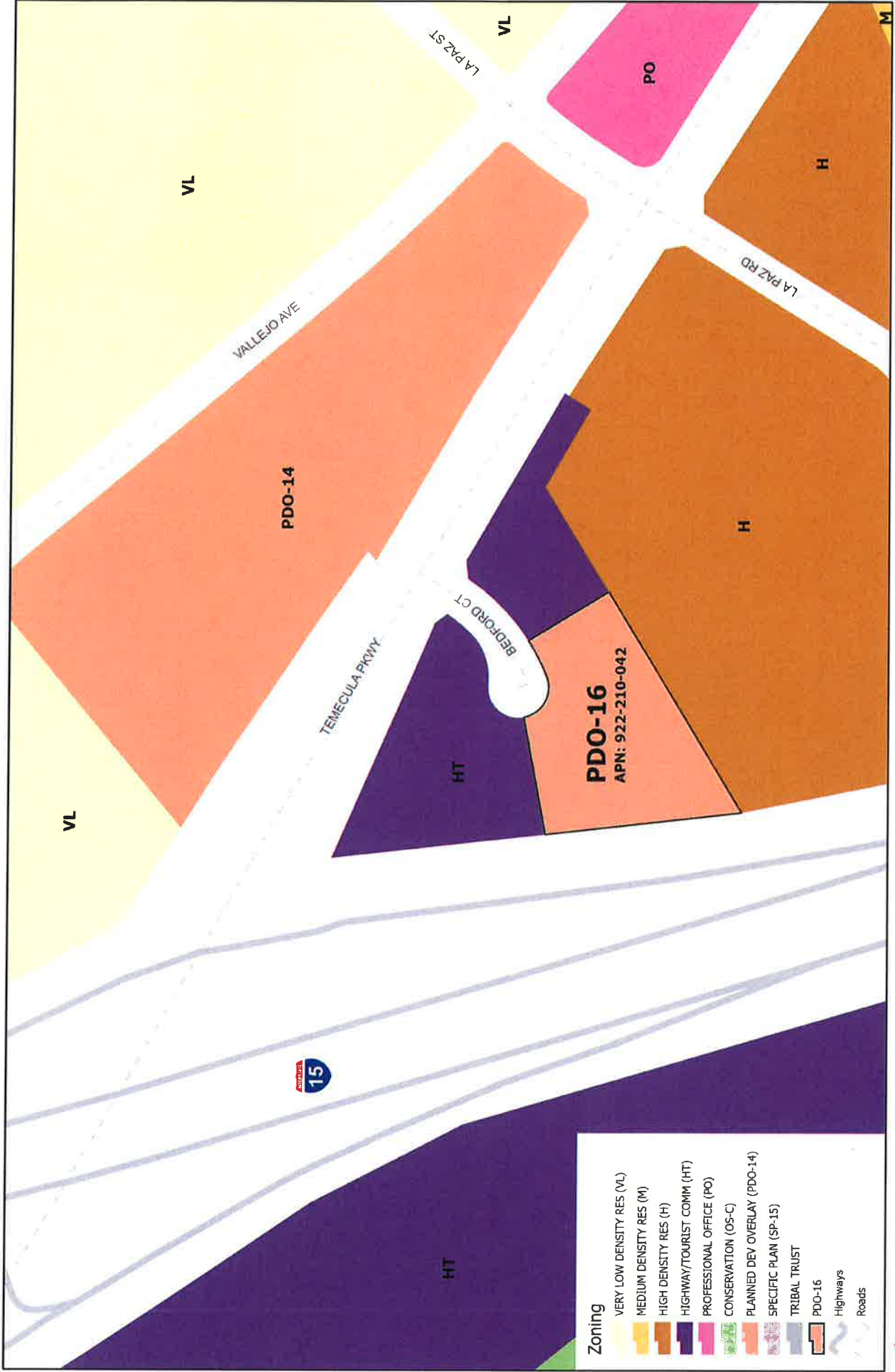
Existing 6' CMU Wall with 4' Wood Fence (to remain)



FIGURE 12 - WALL AND FENCE PLAN



City of Temecula - PDO-16



The map PDO16.mxd is maintained by City of Temecula GIS. Data and information represented on this map are subject to update and modification. The City of Temecula assumes no warranty or legal responsibility for the information contained on this map. This map is not for reprint or resale. Visit the City of Temecula GIS online at <https://temeculaca.gov/gis>

